

ESL385 Stator - Fitting

Applications: Yamaha YZ125 (96-01) and YZ250 (96-06)

Step 1 Once you take the ignition cover off Are The Replacement Parts Similar? Compare the replacement part to the original. The replacement part should match the mounting hole locations. If not: Double check the application listing with your bike.

Step 2 Remove flywheel and original stator and backing plate from the engine. Please refer to your service manual for specific information if necessary. Note: When removing the existing backing plate mounting screws, apply direct downward force to the screw head to aid in removal). Connect wiring following the diagrams listed for your bike model.

IMPORTANT: CDI Connection Be sure to attach the GREEN/BLUE or PINK wire from the existing CDI unit to the backing plate. This wire was previously connected to the original stator.

WIRE COLOR CONNECTIONS

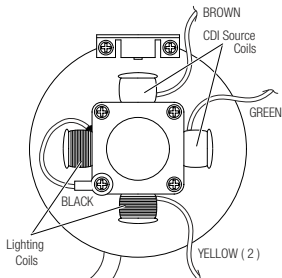
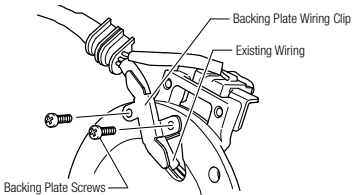
YZ125-YZ250

Black / Red
Green / White
Green / Blue
Black
Headlight

ESL385 Stator Wiring

Brown
Green
Earth Plate - Ground
Earth Plate - Ground
Yellow

BACKING PLATE WIRING CLIP The backing plate has a clip on the back which is used to secure the wiring onto the backing plate. It is very important that the wiring and clip mounts flush to the crank when you install the stator. If for some reason your backing plate does not fit flush, just use your existing clip that was on the original backing plate.



Troubleshooting: Engine will not start: If there is no spark after fitting the assembly, the coil may be installed into the wrong position, or the connections are incorrect or the source coil wires are reversed. Swap the connections, resolder the wires and the engine should start. If the engine still does not start, please refer to your service manual for more electrical system troubleshooting. Note: The OHMS reading for this part may be different than what is listed in the service manual. This is due to the high power winding used in the ElectroSport parts.

The yellow wires are lighting output. Connect one to the lighting system. The other must be grounded. You can run it straight to the headlight. However it is better to use a 12V-AC regulator parallel in circuit.